

# CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)

## CHO RUNWAY 3-21 AND TAXIWAY A PAVEMENT MAINTENANCE

October 2024  
Charlottesville,  
Virginia

RS&H Project No.:  
1054-1892-013

Prepared by RS&H, Inc.  
at the direction of  
Charlottesville Albemarle Airport

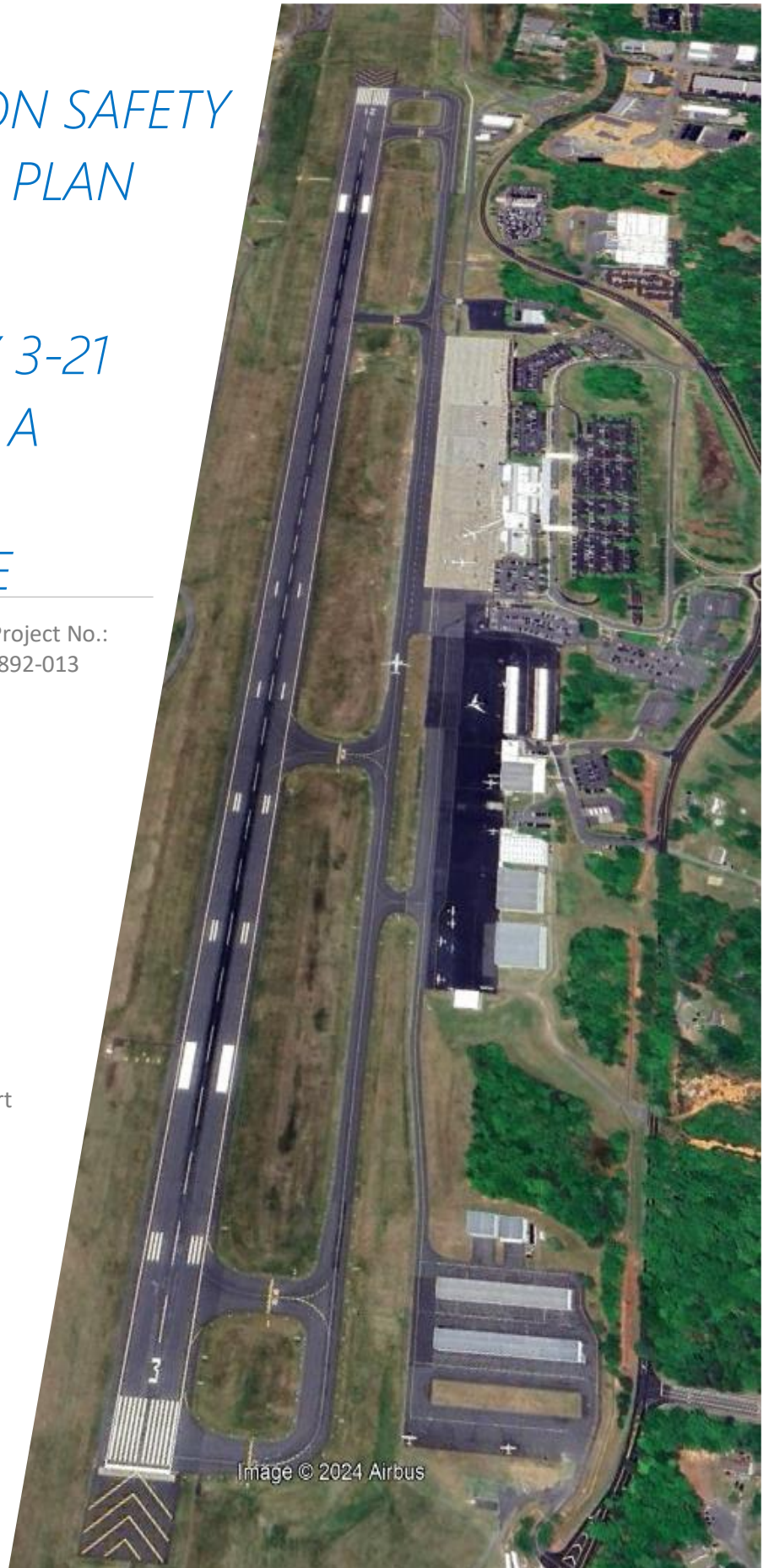


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## 1. INTRODUCTION

Aviation safety is the primary consideration at airports, especially during construction. The airport operator's Construction Safety and Phasing Plan (CSPP) and the contractor's Safety Plan Compliance Document (SPCD) are the primary tools to ensure safety compliance when coordinating construction activities with airport sponsors. These documents identify all aspects of the construction project that pose a potential safety hazard to airport operations and outline respective mitigation procedures for each hazard. The documents will provide the information necessary for the Charlottesville Albemarle Airport (Airport) along with RS&H (Engineer) to conduct airfield inspections and expeditiously identify and correct unsafe conditions during construction. All aviation safety provisions included within the project drawings, contract specifications and other related documents will also be reflected in the CSPP and SPCD. This CSPP is a separate document written to correspond with the safety and security requirements pursuant to FAA Advisory Circular 150/5370-2G: *Operational Safety on Airports During Construction*.

## 2. SCOPE

The project at The Charlottesville Albemarle Airport (CHO) consists of the preventative pavement maintenance of Runway 3-21 and its associated taxiways. Maintenance includes crack sealing, a 2-inch mill and overlay, rubber removal, and paint remarking. Maintenance is required due to the runway and taxiway showing signs of distress and to prevent the need for a future mill and overlay of the entire runway and taxiway, which would require extended Runway closure. Runway 3-21 underwent a mill and overlay in 2017 and minimizing runway closures is a priority. The limits of the project area are identified in the drawings contained within Appendix A.

The impact of construction on the Airport Operations Area (AOA) is discussed in Section 3: Plan Requirements and is also graphically presented in the Safety Plan drawing set in Appendix A. These drawings will also be part of the construction contract documents available to the contractors for bidding.

### 3. PLAN REQUIREMENTS

According to AC 150/5370-2G, the CSPP should address the plan requirements presented in this section. Specific construction details and methods that cannot be determined at this stage will be included in the SPCD submitted by the contractor.

#### A. Coordination

The Engineer has been coordinating project construction impacts on the operation of the Airport with the Airport Manager, Airport Operations and FAA through the design process. Once construction begins, weekly meetings will be held with Airport Operations. The FAA and Virginia Department of Aviation (DOAV) are invited to attend any of the weekly progress meetings. The following coordination requirements for the project will be conducted in accordance with AC 150/5370-12B: *Quality Management for Federally Funded Airport Construction Projects*.

##### 1. Pre-Bid Conference

The Airport and Engineer will conduct a pre-bid meeting to assist interested prime contractors and their subcontractors with construction material requirements, schedule, airport procedures and other items required by the contract. The pre-bid meeting will be held approximately two to three weeks prior to the bid opening date and will be mandatory for all contractors planning on submitting a bid. During the pre-bid meeting, the Engineer will discuss with the prime contractors the pertinent information regarding the development and submittal of a SPCD for review and approval.

##### 2. Pre-Construction Meeting

A pre-construction conference led by the Engineer will be held to discuss airport safety during construction, testing frequencies and quality, project security, labor requirements, quality control, and other issues as identified by the Engineer or Airport. The pre-construction meeting will be held after the notice of award has been issued but prior to issuance of Notice-to-Proceed. The expected participants for this meeting will include:

- Airport Manager
- Airport Operations
- Engineer
- Testing Laboratory
- Contractor
- Subcontractors
- Air Traffic Control Tower (ATCT) Manager
- FAA Washington Airport District Office (ADO)
- Virginia Department of Aviation (DOAV)

##### 3. Progress Meetings

The Contractor shall conduct progress meetings at the Project Site at regularly scheduled times on a weekly interval. Notify the Owner and the Architect of scheduled meeting dates. Coordinate dates of meetings with preparation of the payment request and in conjunction with any coordination meetings. In addition to representatives of the Owner and the Architect, each subcontractor, supplier, or other entity concerned with current progress or involved in planning, coordination, or performance of future activities shall be represented at these meetings.

4. *Scope or Schedule Changes*

If a need arises to modify the scope or schedule during construction, the CSPP may require revision with review and approval by the Engineer, Airport and FAA. If there is a need for revisions, the FAA Washington ADO and DOAV will be notified in advance of the proposed changes. Any changes to CSPP will require approval from the Airport, FAA and DOAV prior to implementation.

5. *FAA ATO Coordination*

There is an Air Traffic Control Tower (ATCT) at the Airport. The Engineer will coordinate, through Airport Operations, construction phasing that impacts with the ATCT's Air Traffic Manager.

## **B. Phasing**

The development of the construction phasing has been coordinated with Airport Operations. The specific elements of the construction phasing have been incorporated into the project design are reflected in the contract drawings and specifications.

1. *Phase Elements*

The construction duration is 10 calendar nights. All work will be performed in one phase between the hours of 12:00 AM and 5:00 AM. Refer to the drawings in Appendix A for a graphical description of the work areas.

- i. Phase 1 – Construction will last 10 nights and will require mobilization and demobilization each night. Lighted runway closure crosses and barricades will need to be placed before construction can begin. Construction will consist of crack sealing throughout Runway 3-21 and associated taxiways, a 2-inch mill and overlay in areas requiring patching, rubber removal throughout Runway 3-21, and remarking within the RSA on the Runway and Taxiway.

2. *Construction Safety Drawings*

The drawings specifically indicating operational safety procedures and methods in the impacted areas have been provided in this CSPP (Appendix A) and have been incorporated into the contract documents plan set.

**C. Areas and Operations Affected by Construction Activity**

1. *Identification of Affected Areas*

Identifying areas and operations affected by construction will assist in determining potential safety hazards. These areas are outlined on the specific Safety Plans found in Appendix A. However, AC 150/5370-2G identifies six areas of concern that should be addressed in the CSPP:

- i. Closing, or Partial Closing, of Runways, Taxiways and Aprons – Runway 3-21 and Taxiway A will be closed during construction.
- ii. Closing of Aircraft Rescue and Fire Fighting Access Routes – The Contractor will be advised that airport personnel and fire rescue vehicles always have the right-of-way during construction.
- iii. Closing of Access Routes used by Airport and Airline Support Vehicles – The Contractor will be instructed to work with the Engineer to ensure Airport Operations always has access to all areas of the airfield during construction.
- iv. Interruption of Utilities, Including Water Supplies for Fire Fighting – The Contractor will be instructed to work with the Engineer to ensure any interruption of utilities is properly managed and reported to Airport Operations.
- v. Approach/Departure Surfaces Affected by Heights of Objects – There are no anticipated impacts to approach or departure surfaces from equipment heights during construction.
- vi. Construction Areas – These areas include the project area, contractor staging and equipment parking/stockpile areas and contractor haul routes near the runway, taxiways, or aprons (Appendix A).

2. *Mitigation of Effects*

The CSPP has established specific requirements and procedures necessary to maintain the safety and efficiency of airport operations. All coordination involving airport operations during construction will flow through the Engineer. All required NOTAMs will

be issued by Airport Operations.

- i. Temporary Changes to Runway and/or Taxi Operations – No temporary changes to Runway and/or Taxi operations are anticipated.
- ii. Detours for ARFF and Other Airport Vehicles –The Contractor will be advised that airport personnel and fire rescue vehicles always have the right-of- way during construction.
- iii. Maintenance of Essential Utilities –The Contractor shall bear full responsibility for the protection of utilities. Where required, the FAA shall locate all their underground utilities. The Contractor will verify the exact locations of essential utilities at least 72 hours prior to beginning construction activity. Utility requests for Airport and FAA facilities shall be submitted through the Engineer on a 7-day minimum notice. All locations of cabling and other underground utilities will be marked prior to any excavation.

#### **D. Protection of Navigation Aids (NAVAIDs)**

NAVAIDs on Runway 3-21 will not be impacted.

#### **E. Contractor Access**

The CSPP details those areas within the AOA to which the Contractor will have access and how contractor personnel will access the project work area.

##### *1. Contractor Staging Areas*

The construction staging areas are shown on the plans and can be found in Appendix A. When not in use, stockpiled equipment staged within the area construction sites shall not impact airport operations and should be coned off.

##### *2. Location of Stockpiled Construction Materials*

Stockpiled construction materials and equipment storage shall generally be within the main Contractor staging area. Stockpiled equipment and construction material location should be approved by the engineer and by airport operations. Stockpiled equipment and construction material located outside of the airport building shall not penetrate the 40 CFR Part 77 airspace surfaces. For additional information regarding Part 77 imaginary surfaces, please see the Safe, Efficient Use and Preservation of the Navigable Airspace (Part 77) document found on the FAA website. The Contractor is expected to manage stockpiles so that they do not attract wildlife (Refer to Section F). The Contractor shall manage stockpiles so that they do not create foreign object debris (FOD) (Refer to



Section G). Stockpiles shall be marked and lit as noted in Section P.

3. *Vehicle and Pedestrian Operations*

The CSPP includes specific vehicle and pedestrian requirements. Controlled vehicle and pedestrian access routes for airport construction projects will prevent inadvertent or unauthorized entry of persons, vehicles, or animals onto the AOA. Regarding vehicle or pedestrian operations, the CSPP includes the following vehicle and pedestrian requirements for the project:

- i. Access to Airport Operations Area (AOA): No person shall enter the AOA, or any other restricted area, except authorized personnel assigned to duty therein and personnel escorted by an appropriately badged escort.
- ii. Construction Site Parking: All construction vehicles are restricted to the immediate work area as specified on the Safety Plans. These areas include haul routes, the designated contractor's staging area and the work area itself.
- iii. Construction Equipment Parking: Contractor employees will park and service all construction vehicles in the designated areas as agreed upon by the engineer and airport operations.
- iv. Access and Haul Roads: The contractor's access to the construction site and haul roads are depicted on the Safety Plans. The Contractor will not be permitted to utilize other access or haul roads unless approved by the Airport. Access routes used by contractor vehicles will be clearly marked to prevent inadvertent entry to areas open to airport operations.
- v. Marking and Lighting of Vehicles: All marking and lighting of construction vehicles will be in accordance with AC 150/5210-5D *Painting, Marking and Lighting of Vehicles Used on an Airport*.
- vi. Maintenance of Secured Area of the Airport: The Contractor and subcontractors will take care to maintain security during construction when access points are created onto the airfield to permit the passage of construction vehicles or personnel.
  - a. The Airport will meet standards contained within CRF Part 1542, Airport Security, to the extent practicable for a general aviation facility.

## **F. Wildlife Management**

The Contractor will be reminded by the Engineer to carefully control and continuously remove waste or loose materials that might attract wildlife. Contractor personnel will be aware of and avoid construction activities that can create wildlife hazards on airports, such as:

1. *Trash*

The Contractor will secure and clean up all FOD upon observation. All food scraps from construction personnel activity will be collected and properly disposed of so as not to attract wildlife.

2. *Standing Water*

All efforts will be made to prevent water from collecting and pooling for more than a 24- hour period.

3. *Tall Grass and Seeds*

The sodding and topsoiling around the constructed pavements have been designed to allow access for lawn mowers to prevent the growth of tall grasses.

4. *Poorly Maintained Fencing and Gates*

The Contractor shall immediately notify the Engineer and Airport Operations if project related activities compromise any portions of the airport perimeter fencing.

5. *Disruption of Existing Wildlife Habitat*

The proposed work will not impact any designated wildlife habitat areas. The Engineer will instruct contractor to immediately notify the Engineer if any contractor personnel see any wildlife in or around work area.

## **G. Foreign Object Debris (FOD) Management**

Waste and loose materials, commonly referred to as FOD, can cause damage to aircraft landing gears, propellers, and jet engines. The contractor will be instructed by the Engineer to not leave or place FOD on or near active aircraft movement areas. Materials capable of creating FOD will be continuously removed during the construction. To mitigate FOD, all construction material, contractor field trailers, personal vehicle parking, and trash dumpsters will be in the construction staging area and not the work area. Refer to the Safety and Phasing plan sheet located in Appendix A for the staging area location. Contractors shall provide their own equipment for vehicle and construction equipment washing and clean up. The Contractor is required to maintain a 25-minute response time, 24 hours a day and 7 days a week, to address FOD issues identified by Airport Operations.



## H. Hazardous Materials (HAZMAT) Management

All spills must be reported to Airport Operations at (434) 973-8342. The Contractor will be prepared to expeditiously contain and clean-up spills resulting from fuel or hydraulic fluid leaks from construction vehicles and equipment. The Engineer will require special procedures for the transport and handling of other hazardous materials on the Airport. The Contractor will be required by the Engineer to develop and implement spill prevention and response procedures for vehicular operations. These procedures shall be outlined in the contractor's SPCD.

## I. Notification of Construction Activities

The CSPP and SPCD detail procedures for the immediate notification of airport users and the FAA of any conditions adversely affecting the operational safety of the Airport. The following is information and procedures for such notification:

### 1. *Maintenance of a List of Responsible Representatives / Points of Contact*

A list of responsible representatives and their contact numbers will be maintained in this document and distributed to the Contractor. Any changes in personnel will be updated immediately.

Charlottesville Albemarle Airport  
Contact: Jason Burch  
100 Bowen Loop, Suite 200  
Charlottesville, VA 22911  
(434) 973-8342  
jburch@gocho.com

### 2. *Notices to Airmen (NOTAM)*

For the Contractor to operate within Airport property, appropriate NOTAMs will be issued by Airport Operations (with guidance from the Engineer). These notices provide information on closed, limited, or hazardous conditions to airmen and users of the Airport. A 72-hour notice is required for issuance of a proper NOTAM. All construction operations will be closely coordinated with the Engineer for NOTAM issuance.

### 3. *Emergency Notification Procedures*

In the event of an emergency, the Contractor will immediately notify the Engineer first. The Engineer will contact the appropriate resolution personnel. If the emergency requires immediate medical attention, the contractor will call 911, followed by notifying Airport Operations at (434) 973-8342.

4. *Coordination with Aircraft Rescue and Fire Fighting (ARFF) Personnel*

Fire rescue service is provided by the Airport. The Contractor will detail procedures (through the Engineer) with Airport Operations if construction requires:

- i. The deactivation and subsequent reactivation of water lines or fire hydrants, or
- ii. The rerouting, blocking and restoration of emergency access routes, or
- iii. The use of hazardous materials on the airfield

5. *Notification to the FAA*

The following are procedures for coordinating with FAA:

- i. Part 77: Any person proposing construction or alteration of objects that affect navigable airspace, as defined in Part 77, will notify the FAA. This includes construction equipment and proposed parking areas for this equipment (i.e. cranes, graders, other equipment) on airports. FAA Form 7460-1, *Notice of Proposed Construction or Alteration*, can be used for this purpose and submitted to the appropriate FAA Airports Regional or District Office.
- ii. Part 157: With some exceptions, Title 14 CRF Part 157, Notice of Construction Alteration, Activation, and Deactivation of Airports, requires that the airport operator notify the FAA in writing whenever a non-Federally funded project involves construction of a new airport; the construction, realigning altering, activating, or abandoning of a runway, landing strip, or associated taxiway; or the deactivation or abandoning of an entire airport. Notification involves submitting FAA Form 7480-1, Notice of Landing Area Proposal, to the nearest FAA Airports Regional or District office. It is anticipated that Part 157 notifications will not be required for this project.
- iii. NAVAIDS: For emergency (short-notice) notification about impacts to both airport-owned and FAA-owned NAVAIDS, contact Airport Operations as well as the Operations Control Center at (434) 973-8342.
  - a. Airport-owned – If construction operations require a shutdown of more than 24 hours, or more than 4 hours daily on consecutive days, notification will be provided to Airport Operations at least 45 days prior to the required facility shutdown.

- b. FAA-owned – Airport Operations will notify the appropriate FAA ATO Service Area Planning and Requirements (P&R) Group a minimum of 45 days prior to implementing an event that causes impacts to the NAVAIDs (Impacts to FAA equipment covered by a Reimbursable Agreement (RA) do not have to be reported by the airport operator). Coordinate work for an FAA-owned NAVAID shutdown with the local FAA ATO/Technical Operations office, including any necessary reimbursable agreements and flight checks. Detail procedures that address unanticipated utility outages and cable cuts that could impact FAA NAVAIDs. In addition, provide 7-day notice to schedule the actual shutdown. A RA will be completed for the NAVAID impacts prior to the construction Notice-to-Proceed.

## **J. Inspection Requirements**

### *1. Daily (or more frequent) Inspections*

Inspections should be conducted at least daily, but more frequently if necessary, to ensure conformance with the CSPP. A sample checklist is provided in Appendix 4 of AC 150/5370-2G. In addition to the contractor's required inspections, the work area is subject to inspection by representatives of Airport Operations, Transportation Security Administration (TSA), FAA, and other affected agencies to ensure compliance with the CSPP and the SPCD. The Engineer will have full-time inspectors monitoring activity throughout construction.

### *2. Final Inspections*

A final inspection with the Engineer, Airport, DOAV and FAA will take place prior to final acceptance of the project. Punch list inspection(s) will precede the final inspection.

## **K. Underground Utilities**

The Contractor shall bear full responsibility for the protection of utilities. Where required, the FAA shall locate their underground utilities within the project area. The Contractor will verify the exact locations of essential utilities prior to construction, at least 72 hours prior to beginning construction activity. Utility requests for Airport and FAA facilities will be submitted through the Engineer on a 7-day minimum notice. It should also be noted that excavation in certain areas may require a soft dig. All locations of cabling and other underground utilities will be marked prior to any excavation.

## **L. Penalties**

Penalties for non-compliance with the plans and procedures outlined in this document vary from verbal and written warnings to suspension of the project until compliance is achieved.

## **M. Special Conditions**

There are no additional special conditions involved with the project that affect airport operations.

## **N. Runway and Taxiway Visual Aids (Marking, Lighting, and Signs)**

The areas where aircraft will be operation during construction will be clearly and visibly separated from construction areas, including closed airfield pavements. Throughout the duration of the construction project, the contractor will verify that these areas always remain clearly marked and visible, and that marking, lighting, and signs remain in place and operational.

### *1. General*

Airport markings, lighting, and signs will be clearly visible to pilots, not misleading, confusing, or deceptive. All will be secured in place to prevent movement by prop wash, jet blast, wing vortices or other wind currents and constructed of materials that would minimize the damage to an aircraft in the event of inadvertent contact.

### *2. Markings*

- i. Permanently Closed Taxiways – No existing pavements will be permanently closed by this project.
- ii. Partially Closed Taxiways – No taxiways will be partially closed by this project.
- iii. Temporarily Closed Taxiways – Taxiway A will be temporarily closed by this project.

### *3. Lighting and Visual NAVAIDs*

Runway 3-21 and Taxiway A will be deenergized while they are closed.

### *4. Signs*

No airfield guidance signs will be impacted during this project. Airfield guidance signs shall conform to AC 150/5345-44K *Specification for Runway and Taxiway Signs* and AC 150/5340-18H, *Standard for Airport Sign Systems*. If at any point during construction a sign does not service its normal function, it will be covered or removed to prevent misdirecting pilots.

## **O. Marking and Signs for Access Routes**

Pavement markings and signs for construction personnel shall conform to AC 150/5340-18H and, to the extent practicable, with the Federal Highway Administration Manual on Uniform Traffic Control Devices (MUTCD) and/or state highway specifications. Signs adjacent to areas used by

aircraft will comply with the frangibility requirements of AC 150/5220-23, *Frangible Connections*, which may require modification to size and height guidance in the MUTCD.

## **P. Hazard Marking and Lighting**

Hazard marking, lighting and signage warns and prevents pilots from entering areas closed to aircraft and prevents construction personnel from entering areas open to aircraft. There will be warning indicators installed for any area affected by construction that is normally accessible to aircraft, personnel or vehicles. Hazard marking and lighting will also be specified to identify open manholes, small areas under repair, stockpiled materials, waste areas and areas subject to jet blast. The contractor will install taxiway closure markers and Type-1 low profile barricades as specified in the contract plans and specifications. The Contractor will be responsible for maintaining proper positioning of all barricades, as well as ensuring the barricades remain full of water while in use. The Airport will provide the Contractor with lighted runway closure crosses for use during the runway closure. The Contractor will be directed to have replacement lights on-site and shall replace lights within one hour of notification by the Engineer or Airport.

## **Q. Protection**

The protection of runway and taxiway safety and object free areas will be an on-going requirement through the duration of the project. These requirements will place limitations on the location and height of equipment and stockpiled materials.

### **1. Runway Safety Area (RSA)**

The RSA for Runway 3-21 is 500 feet wide (250 feet either side of runway centerline) and extends 1,000 feet beyond each runway end. No contractor personnel or equipment will be allowed within the RSA when the runway is open to air traffic.

### **2. Runway Object Free Area (ROFA)**

The ROFA for Runway 3-21 is 800 feet wide (400 feet either side of runway centerline) and extends 1,000 feet beyond each runway end. Work may occur within the ROFA while the runway is open, provided the appropriate NOTAMs are in place and the ATCT has been notified. This is not expected to frequently occur due to the phasing scheme developed for this project, but may for minor work items.

### **3. Taxiway Safety Area (TSA)**

The TSA for parallel Taxiway A is 118 feet wide (59 feet either side of taxiway centerline). No contractor personnel or equipment will be allowed within the TSA of a taxiway open to air traffic.

### **4. Taxiway Object Free Area (TOFA)**

The TOFA for parallel Taxiway A is 171 feet wide (85.5 feet either side of taxiway

centerline). Work may occur within the TOFA while the taxiway is open, provided the appropriate NOTAMs are in place and the ATCT has been notified.

*5. Runway Obstacle Free Zone (OFZ)*

The ROFZ for Runway 3-21 is 400 feet wide (200 feet either side of runway centerline) and extends 200 feet beyond each runway end. The surface rises vertically to a point 150 feet above the airport elevation. Work for this project will not impact the ROFZ.

*6. Runway Approach / Departure Surfaces*

No approach or departure surfaces will be affected by this project.

**R. Other Limitations on Construction**

The following are other limitations during construction:

*1. Prohibitions*

- i. Tall Equipment: No equipment greater than 15 feet in height will be allowed by the Engineer for use onsite unless a 7460-1 determination letter has been issued by the FAA Washington ADO for such equipment.
- ii. Open Flame Welding: There will be no use of open flame welding or torches unless fire safety precautions are provided, and the Airport has approved their use.
- iii. Electrical Blasting Caps: There will be no use of electrical blasting caps on or within 1,000 feet of Airport property.
- iv. Flare Pots: Not allowed within the AOA.

*2. Restrictions*

- i. Construction Suspension Required During Specific Airport Operations – The Airport will notify the Contractor at the pre-construction meeting if any events during construction will require a shut-down. This is not anticipated, however.
- ii. Day or Night Construction Restrictions – The Airport will notify the Contractor at the pre-construction meeting to any restrictions regarding night work.

**S. Construction Contractor's Responsibilities to Comply with this CSPP**

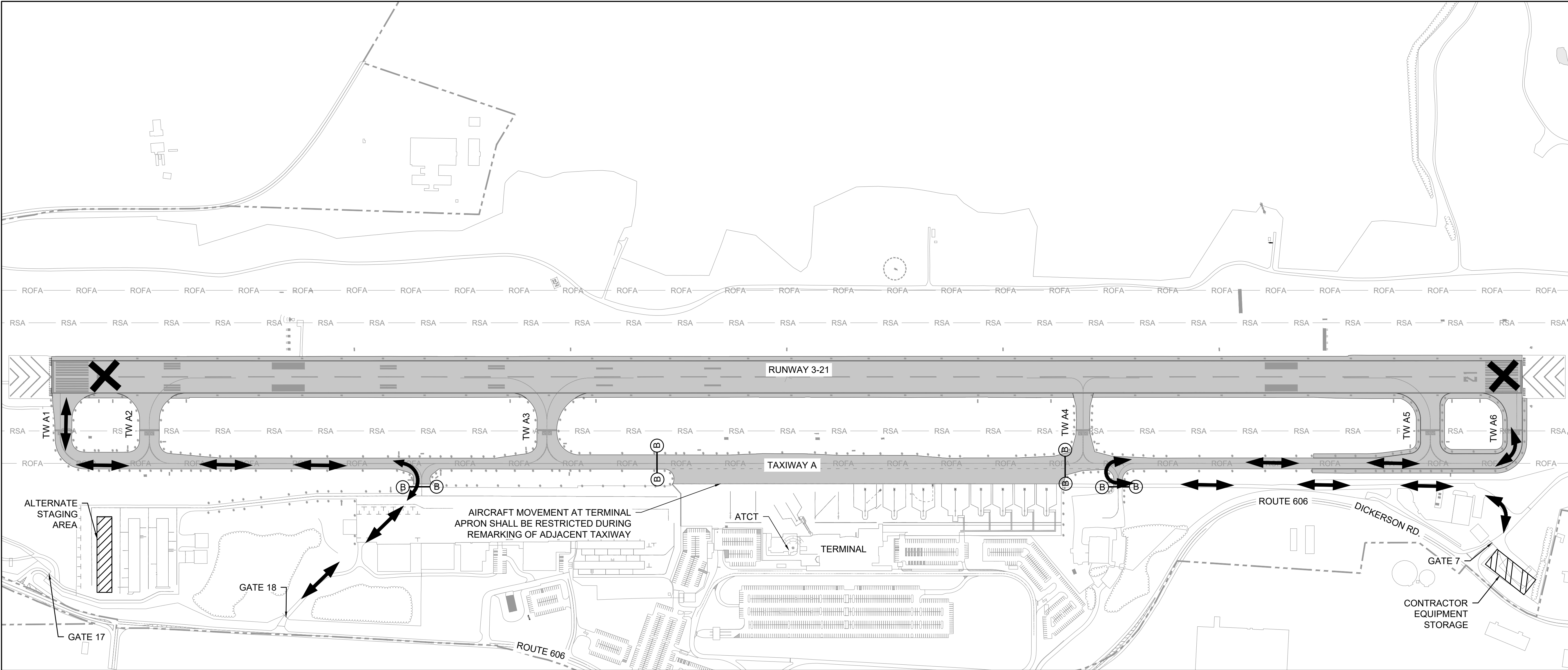


The construction contractor is responsible for the following:

- The Contractor will submit a SPCD to the Airport describing how it will comply with the requirements of the CSPP. The Contractor shall provide any details that could not be determined before the contract is awarded. The SPCD will include a certification statement by the Contractor that indicates it understands the operation safety requirements of the CSPP and it asserts it will not deviate from the approved CSPP and SPCD unless written approval is granted by the Airport. Any construction practice proposed by the Contractor that does not conform to the CSPP and SPCD may impact the Airport's operational safety and will require a revision to the CSPP and SPCD and re-coordination with the Engineer, Airport and FAA in advance.
- Always keep available copies of the CSPP and SPCD for reference by the Airport, and by subcontractors and contractor employees.
- Ensure that construction personnel are familiar with safety procedures and regulations at the Airport. Provide a point of contact to Airport Operations and the Engineer who will coordinate an immediate response to correct any construction-related activity that may adversely affect the operational safety of the Airport.
- Identify in the SPCD the Contractor's on-site employees responsible for monitoring compliance with the CSPP and SPCD during construction. At least one of these employees will be on-site whenever active construction is taking place.
- Conduct inspections with frequency to ensure construction personnel comply with the CSPP and SPCD and that there are no altered construction activities that could create potential safety hazards.
- Restrict movement of construction vehicles and personnel to permitted construction areas by flagging, barricading, or providing escorts, as appropriate and as specified.
- Ensure that no Contractor employees, employees of subcontractors or suppliers, or other persons enter any part of the air operations area (AOA) from the construction site unless authorized.
- Ensure prompt submittal through Airport Operations of Form 7460-1 for the purpose of conducting an aeronautical study of contractor equipment such as tall cranes, concrete pumps, etc., stockpiles and haul routes when different from cases previously filed by the Engineer and Airport. The FAA encourages online submittal of forms for expediency.

## *Appendix A – Drawing Set*

Drawing: P:\Charlottesville\_Albemarle\10221892\_CHO\_Civil\_On\_Call\_2022\_2026\10541892013\03.00 Project Execution\03.05 Dwg\_Models\CAD\CHO-PMT-Construction Safety Phasing Plan.dwg - Plotted on: 5/16/25 - Plotted by: Blaine, Jason



PROJECT FEATURES

- THIS PROJECT INCLUDES:
  - CRACK SEALING THROUGHOUT RUNWAY 3-21 AND TAXIWAY A AND ASSOCIATED TAXIWAY CONNECTORS
  - 2-INCH MILL AND OVERLAY PATCHING
  - RUBBER REMOVAL (RUNWAY)
  - RUNWAY AND TAXIWAY REMARKING
- ALL WORK SHOWN ON THIS SHEET SHALL BE INCIDENTAL TO ITEM X-102, SAFETY AND SECURITY ITEMS AND C-105, MOBILIZATION.
- OWNER WILL PROVIDE AND MAINTAIN RUNWAY CLOSURE LIGHTED X's

PROJECT PHASING

ADMINISTRATIVE PHASE:  
THIS PHASE INCLUDES SHOP DRAWING REVIEW AND CONTRACTOR MATERIAL ACQUISITION. NO CONSTRUCTION OR MOBILIZATION OCCURS DURING THE ADMINISTRATIVE PHASE - 30 DAYS.

WORK HOURS - 10 NIGHT(S) (12:00 AM - 5:00 AM)  
THIS WORK INCLUDES MOBILIZATION, LIGHTED CLOSURE CROSS PLACEMENT, PAVEMENT REPAIR PATCHING, BARRICADE PLACEMENT, FULL APPLICATION OF PAINT IN AREAS SHOWN ON PLANS, RUNWAY GROOVING, AND DEMOBILIZATION.

AIRFIELD CLOSURE

- CONTRACTOR SHALL STAGE VACUUM OR SWEEPING EQUIPMENT ON-SITE. CONTRACTOR SHALL KEEP BOTH ENDS OF THE RUNWAY CLEAN OF FOD, MUD AND OTHER DEBRIS WHILE USING BOTH ENDS OF THE RUNWAY TO ACCESS THE WORK AREA.
- CONTRACTOR SHALL PLACE OWNER-FURNISHED RUNWAY CLOSURE CROSSES AT THE BEGINNING OF THE CLOSURE AND REMOVE AT THE END OF THE CLOSURES AFTER APPROVAL FROM AIRPORT OPERATIONS. CONTRACTOR TO ENSURE AIRFIELD IS CLEARED, FREE OF DEBRIS, MATERIALS, AND EQUIPMENT AT THE END OF WORK EACH NIGHT.
- CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS EACH MORNING BEFORE THE AIRPORT OPENS.

LEGEND

- CONTRACTOR HAUL ROUTES
- WORK AREA DURING AIRPORT CLOSURE
- RSA EXISTING RUNWAY SAFETY AREA
- ROFA EXISTING RUNWAY OBJECT FREE AREA
- RUNWAY CLOSURE CROSS
- LOW PROFILE BARRICADE

0250500

Feet

PRINT IN COLOR

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CHARLOTTESVILLE  
ALBEMARLE  
AIRPORT  
(CHO)

RUNWAY 3-21 AND  
TAXIWAY A  
PAVEMENT  
MAINTENANCE

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: MARCH 2025  
REVIEWED BY: CCA  
DRAWN BY: LSB / RKH  
DESIGNED BY: RSY  
RS&H PROJECT NUMBER  
10541892013  
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SHEET TITLE

CONSTRUCTION  
SAFETY  
AND PHASING  
PLAN

SHEET NUMBER  
G004  
SHEET 4 OF 19  
BID DOCUMENTS



Drawing: P:\Charlottesville\_Albemarle\10221892\_CHO\_Civil\_On\_Call\_2022\_2026\10541892013\03.00 Project Execution\03.05 Dwggs\_Models\CAD\CHO-PWT-Constr Safety Security Notes Details.dwg - Plotted on: 5/16/25 - Plotted by: Blaine, Jason

SAFETY

1.

THE CONTRACTOR SHALL ACQUAINT HIS SUPERVISORS AND EMPLOYEES OF THE AIRPORT ACTIVITY AND OPERATIONS THAT ARE INHERENT TO CHARLOTTESVILLE ALBEMARLE AIRPORT AND SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY AS SPECIFIED IN SAFETY NOTE 10. CONTRACTOR SHALL SUBMIT A SAFETY PLAN TO THE AIRPORT WHICH WILL BE MONITORED ACCORDINGLY. CONSTRUCTION SHALL NOT COMMENCE UNTIL THIS PLAN IS APPROVED BY THE AIRPORT. THE SAFETY PLAN SHALL BE SUBMITTED PRIOR TO THE NTP.
2.

CONTRACTOR TO PROVIDE A DESIGNATED ON-CALL EMPLOYEE WHO IS AVAILABLE TO QUICKLY RESPOND TO THE PROJECT SITE 24-HOURS A DAY.
3.

ALL CONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT IN OR NEAR THE ACTIVE AIRCRAFT OPERATIONS AREA (AOA) SHALL BE REQUIRED TO HAVE THEIR COMPANY LOGO ON BOTH SIDES OF THE VEHICLE AND SHALL DISPLAY IN FULL VIEW A FLASHING AMBER (YELLOW) DOME-TYPE LIGHT VISIBLE IN ALL DIRECTIONS ABOVE THE VEHICLE OR A 3' X 3' OR LARGER, ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE, (SEE CONSTRUCTION SAFETY FLAG DETAIL ON SHEET G004), AND ESCORTED UNDER THE CONTROL OF ONE CONTRACTOR MOBILE (TWO-WAY) RADIO OPERATOR ON THE JOB AT ALL TIMES. ANY VEHICLE OPERATING IN OR NEAR THE ACTIVE AOA DURING THE HOURS OF DARKNESS SHALL BE EQUIPPED WITH A FLASHING AMBER (YELLOW) DOME LIGHT, MOUNTED ON TOP OF THE VEHICLE AND BE OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES. ALL RADIO EQUIPPED VEHICLES SHALL COORDINATE ATCT CALL SIGNS WITH AIRPORT OPERATIONS PRIOR TO OPERATING ONSITE.
4.

ALL NON-RADIO EQUIPPED CONTRACTOR VEHICLES THAT ARE REQUIRED TO OPERATE ON, NEAR OR ACROSS ACTIVE RUNWAYS, TAXIWAYS, APRONS, AND RUNWAY APPROACH AND PROTECTION ZONES SHALL DO SO UNDER THE DIRECT CONTROL OF A RADIO EQUIPPED ESCORT VEHICLE. ESCORT VEHICLES, OPERATORS, AND RADIO EQUIPMENT SHALL BE APPROVED BY THE AIRPORT OPERATIONS STAFF. ESCORT VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED IN PARAGRAPH NO. 2 ABOVE AND SHALL BE EQUIPPED WITH AN APPROVED AVIATION BAND RADIO. RADIO EQUIPPED VEHICLES SHALL CONTINUOUSLY MONITOR GROUND CONTROL FREQUENCY 121.9 MHZ. IF WORK IS PERFORMED DURING PERIODS OF TIME WHEN THE AIR TRAFFIC CONTROL TOWER IS CLOSED, A FREQUENCY OF 133.1 MHZ SHALL BE MONITORED. ALL AIRCRAFT TRAFFIC ON RUNWAYS, TAXIWAYS AND APRONS SHALL HAVE PRIORITY OVER CONTRACTOR'S TRAFFIC.
5.

NO RUNWAY, TAXIWAY, APRON OR AIRPORT ROADWAY SHALL BE CLOSED WITHOUT WRITTEN APPROVAL OF AIRPORT OPERATIONS STAFF. TO ENABLE NECESSARY "NOTICES TO AIRMEN" (NOTAM) OR ADVISORIES TO AIRPORT SERVICES OR TENANTS, A MINIMUM OF 7 DAYS WRITTEN NOTICE REQUESTING CLOSING SHALL BE DIRECTED TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) WHO WILL COORDINATE THE REQUEST WITH AIRPORT OPERATIONS STAFF.
6.

ANY CONSTRUCTION ACTIVITY WITHIN 250 FEET OF AN ACTIVE RUNWAY CENTERLINE OR 85.5 FEET FROM AN ACTIVE TAXIWAY CENTERLINE OR OPEN EXCAVATIONS IN EXCESS OF THREE INCHES DEEP WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED BY AIRPORT OPERATIONS. CLOSURE REQUIRES THE SAME PROVISIONS AS PARAGRAPH NO. 4 ABOVE.
7.

OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS APPROVED BY THE RPR AND/OR AIRPORT OPERATIONS STAFF. IF APPROVED, ADEQUATE FIRE AND SAFETY PRECAUTIONS MUST BE TAKEN.
8.

STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLAST OR WIND CONDITIONS IN EXCESS OF TWENTY MILES PER HOUR. STOCKPILE HEIGHT SHALL BE LESS THAN 15 FEET AND OUTSIDE THE RUNWAY AND TAXIWAY OBJECT FREE AREAS. IN ADDITION, STOCKPILED MATERIAL SHALL HAVE SILT FENCE LOCATED AROUND THE MATERIAL TO PREVENT FOD FROM MOVING ONTO THE AIRFIELD PAVEMENTS OR POLLUTING WATERCOURSES. STOCKPILED MATERIAL SHALL NOT PENETRATE THE 7:1 TRANSITIONAL SLOPE OF THE RUNWAY AFFECTED BY GRADING OPERATIONS.
9.

OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL LOCATED IN OR NEAR THE AOA SHALL BE PROMINENTLY MARKED WITH APPROVED FLAGS AND LOW-PROFILE BARRICADES AND LIGHTED BY APPROVED LIGHT UNITS DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS.
10.

DEBRIS, WASTE AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS OR BEING INGESTED IN JET ENGINES SHALL NOT BE ALLOWED ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS. IF THESE MATERIALS ARE OBSERVED TO BE ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS, THEY WILL BE REMOVED IMMEDIATELY AND CONTINUOUSLY DURING CONSTRUCTION. IF THESE MATERIALS ARE OBSERVED AT ANYTIME, THE CONTRACTOR SHALL REMOVE THEM IMMEDIATELY. A BROOM TRUCK OR VACUUM TRUCK (SAFETY NOTE 16) SHALL BE USED TO IMMEDIATELY REMOVE DUST, DEBRIS, OR FOD.
11.

THE CONTRACTOR IS DIRECTED TO COMPLY WITH AND ACQUAINT HIS/HER EMPLOYEES WITH THE FOLLOWING SAFETY GUIDELINES, RELATED MATERIALS AND FAA ADVISORY CIRCULARS:

150/5200-18C "AIRPORT SAFETY SELF-INSPECTION"

150/5210-5D "PAINTING, MARKING & LIGHTING OF VEHICLES USED ON AIRPORTS"

150/5370-2G "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"

150/5370-13A "OFF-PEAK CONSTRUCTION OF AIRPORT PAVEMENTS USING HOT-MIX ASPHALT"
- COPIES OF THESE DOCUMENTS ARE AVAILABLE ONLINE AT:  
[HTTP://WWW.FAA.GOV/AIRPORTS/RESOURCES/ADVISORY\\_CIRCULARS/](http://www.faa.gov/airports/resources/advisory_circulars/)
12.

THE TERM CONTRACTOR MEANS THE PRIME CONTRACTOR, HIS/HER SUBCONTRACTORS, SUPPLIERS, MATERIAL MEN, EMPLOYEES, AND OTHERS WHO WILL PERFORM SERVICES FOR OR IN CONJUNCTION WITH THE PRIME CONTRACTOR.
13.

CONSTRUCTION DURING THE PROJECT MAY BE HALTED AT ANY TIME BY THE RESIDENT PROJECT REPRESENTATIVE, ENGINEER AND/OR AIRPORT OPERATIONS STAFF IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT OPERATIONS OR SAFETY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS. NECESSARY EXTENSIONS IN CONTRACT TIME MAY BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS. HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS.
14.

THE CONTRACTOR SHALL PREPARE SAFETY PLANS SPECIFIC TO DAYTIME AND NIGHTTIME CONSTRUCTION OPERATIONS, AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS. THE CONTRACTOR SHALL ALSO PREPARE A DESTRUCTIVE WEATHER PLAN TO SET FORTH GENERAL GUIDANCE AND INFORMATION FOR THE
- CONTRACTOR TO COORDINATE PREPAREDNESS PLANS WHEN DESTRUCTIVE WEATHER THREATENS THE AIRPORT. THE DESTRUCTIVE WEATHER PLAN FORM IS PROVIDED IN THE SPECIFICATIONS.
15.

ALL COMMUNICATION WITH THE AIRPORT SHALL BE THROUGH THE ENGINEER/RPR.

16.

THE CONTRACTOR SHALL PROVIDE A VACUUM TRUCK AT THE STARTUP OF CONSTRUCTION TO CONTINUOUSLY VACUUM ALL PAVEMENTS AFFECTED BY CONSTRUCTION. THE VACUUM TRUCK SHALL REMAIN ON-SITE FOR THE DURATION OF THE PROJECT AND SHALL BE AVAILABLE AT THE DISCRETION OF THE OWNER TO VACUUM PAVEMENT AREAS ADJACENT TO THE CONSTRUCTION AREAS TO ENSURE NO FOD IS PRESENT ON PAVEMENTS WITHIN 500 FEET OF ANY CONSTRUCTION AREA. THE COST OF THE VACUUM TRUCK SHALL BE INCLUDED IN THE SAFETY AND SECURITY BID ITEM.

17.

TRANSPORTATION: NO PERSONNEL MAY OPERATE VEHICLES ON THE AIRSIDE UNLESS THEY HAVE FIRST COMPLETED AND PASSED AN AIRPORT APPROVED MOVEMENT AREA AND TRAIN THE TRAINER TRAINING COURSES. ALL PERSONNEL OPERATING VEHICLES ON SITE MUST ATTEND AND COMPLETE THE AIRPORT'S MOVEMENT AREA TRAINING COURSE AND TRAIN THE TRAINER COURSE PRIOR TO OPERATING VEHICLES ONSITE. ONLY INDIVIDUALS WHO HAVE COMPLETED THESE COURSES MAY PROVIDE ESCORT OF DELIVERY AND OTHER CONTRACTOR VEHICLES.

18.

THE ENGINEER WILL ARRANGE WITH AIRPORT OPERATIONS FOR INSPECTION PRIOR TO OPENING FOR AIRCRAFT USE ANY RUNWAY OR TAXIWAY THAT HAS BEEN CLOSED FOR WORK, ON OR ADJACENT THERETO, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR.

19.

THE CONTRACTOR SHALL INSTALL ALL REQUIRED BARRICADES AT DESIGNATED PLAN LOCATIONS, RUNWAY CLOSURE MARKERS, HAVE ALL ACCESS GATES GUARDED AND LOCKABLE, HAVE ALL EQUIPMENT EITHER FLAGGED OR FITTED WITH FLASHING AMBER DOME-TYPE LIGHT ON TOP OF THE VEHICLES, INSTALL ALL TEMPORARY TAXIWAY OR RUNWAY LIGHTING AND/OR SIGNS. ALL THESE ITEMS SHALL CONSIST OF THE SAFETY AND SECURITY SYSTEM. THE CONTRACTOR SHALL INSTALL THE COMPONENTS OF THE SYSTEM AT THE APPROPRIATE TIMES AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL INSPECT EVERY ASPECT OF THE SAFETY AND SECURITY SYSTEM ON A DAILY BASIS AND ENSURE ALL COMPONENTS ARE FUNCTIONING PROPERLY. THE RESIDENT PROJECT REPRESENTATIVE (OWNER'S REPRESENTATIVE) SHALL ALSO DAILY INSPECT THE SYSTEM AND IF ANY DEFICIENCIES ARE NOTED, THE CONTRACTOR SHALL HAVE THAT DAY'S PRORATED SAFETY AND SECURITY COST PERMANENTLY DEDUCTED FROM THE CONTRACTOR'S EARNINGS. THE SYSTEM ELEMENTS TO BE INSPECTED WITH DEFICIENCIES NOTED ARE AS FOLLOWS:

\*BARRICADES SET PROPERLY AND ALL FLASHING WARNING LIGHTS OPERATING PROPERLY.

\*ALL CONTRACTOR PERSONNEL AND EQUIPMENT ACCESS GATES MANNED.

\*ALL SECURITY PROCEDURES IN PLACE.

\*ALL EQUIPMENT FLAGGED OR OUTFITTED WITH FLASHING AMBER DOME-TYPE LIGHTS.

\*CONTRACTOR USE OF UNAUTHORIZED AIRPORT ACCESS GATES CHECKED.

\*INSTALLATION OF ALL TEMPORARY TAXIWAY OR RUNWAY LIGHTING AND/OR SIGNAGE.

20.

THE CONTRACTOR SHALL MAKE A CONCERTED EFFORT TO ENSURE ALL SAFETY AND SECURITY ITEMS ARE IN PROPER WORKING ORDER EACH DAY DUE TO THE CONSIDERABLE LIABILITY ASSOCIATED WITH CONSTRUCTION ON AN ACTIVE AIRFIELD.
- SECURITY
1.

GENERAL INTENT: IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS STAFF. THE CONTRACTOR SHALL DESIGNATE TO THE RPR AND AIRPORT OPERATIONS STAFF, IN WRITING, THE NAME OF HIS "CONTRACTOR SECURITY & SAFETY OFFICER (CSSO)." THE CSSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT. THE CSSO SHALL PROVIDE TELEPHONE NUMBERS WHICH ALLOW THE RPR AND/OR AIRPORT OPERATIONS STAFF THE ABILITY TO CONTACT THE CSSO 24 HOURS A DAY.

2.

CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE CSSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.

3.

ACCESS TO THE SITE: CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR GUIDED BY AUTHORIZED CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON THE SITE, AND FOR THE OPERATION AND SECURITY OF THE ACCESS GATE TO THE SITE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE SITE SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNING FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DAISY CHAIN ALL PERMANENT AND TEMPORARY ACCESS GATES TO THE PROJECT SITE AND STAGING AREAS.

4.

MATERIALS DELIVERY TO THE SITE: ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE AS A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT, "DICKERSON ROAD." THE NAME "CHARLOTTESVILLE ALBEMARLE AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INADVERTENTLY.

5.

CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING AND WARNING DEVICES VISIBLE FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT OPERATIONS.
6.

IDENTIFICATION -- PERSONNEL: THE CHARLOTTESVILLE ALBEMARLE AIRPORT RESERVES THE RIGHT TO REQUIRE AIRPORT BADGING FOR CONTRACTOR'S EMPLOYEES DURING THE COURSE OF THIS CONSTRUCTION PROJECT. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE REQUIRED PAPERWORK HAS BEEN COMPLETED BY AIRPORT OPERATIONS AND ALL REQUIREMENTS ARE MET. PAPERWORK SHALL BE SUBMITTED AS DIRECTED BY AIRPORT OPERATIONS STAFF BEFORE ISSUANCE OF ANY BADGE. THE CONTRACTOR AND ITS STAFF ARE RESPONSIBLE FOR ATTENDING TRAINING AND COMPLETING SECURITY BADGE APPLICATIONS, WHICH WILL INCLUDE AIR/GROUND RADIO, TAXIWAY AND AIRPORT FAMILIARIZATION. ESTIMATED TIME FOR COMPLETION OF TRAINING IS ONE (1) HOUR. AIRPORT OPERATIONS SHOULD BE NOTIFIED IMMEDIATELY IF A BADGE IS LOST. REPLACEMENT BADGES COST \$35. CONTRACTOR TO PROVIDE THE ENGINEER/RPR WITH A LIST OF INDIVIDUALS REQUIRING BADGING.

7.

FINES: PAYMENT OF ALL FINES ASSESSED TO CHARLOTTESVILLE ALBEMARLE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FAA REGULATIONS OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

8.

VEHICLE IDENTIFICATION: THE CONTRACTOR, THROUGH THE CSSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE.

9.

VEHICLE PARKING: CONTRACTOR EMPLOYEE VEHICLES SHALL BE RESTRICTED TO THE CONTRACTOR'S EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AOA AT ANY TIME.
- SAFETY PLAN NARRATIVE
- THE SAFETY PLAN SHALL CONSIST OF SHEET G005, ALONG WITH REQUIREMENTS ESTABLISHED IN THE CONTRACT DOCUMENTS AND AS SET FORTH IN AC 150/5370-2G, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- A.

THE CONTRACTOR SHALL FOLLOW ALL GUIDELINES AND REQUIREMENTS ESTABLISHED BY AUTHORITIES HAVING JURISDICTION (EITHER LOCAL, STATE OR FEDERAL) RELATING TO HAZARDOUS MATERIALS (HAZMAT) DURING THE LIFE OF THE CONSTRUCTION.

B.

THE SCOPE OF THIS PROJECT INCLUDES THE MILL AND OVERLAY OF A LOCALIZED AREA OF RUNWAY 3-21, RUNWAY GROOVING IN THAT AREA, TARGETED CRACK SEALING, AND RE-PAINTING OF RUNWAY MARKINGS WITHIN THE PAVEMENT REPAIR AREAS.

C.

FOR RUNWAY CLOSURES, SEE CONSTRUCTION SAFETY PHASING PLAN SHEET G004.

D.

BASIS FOR TEMPORARY DISPLACED THRESHOLDS: NOT APPLICABLE.

E.

BASIS FOR DEVIATING FROM STANDARDS: NOT APPLICABLE.

F.

TYPE AND HEIGHT (NOT-TO-EXCEED) OF CONSTRUCTION EQUIPMENT:

• TRUCKS (DUMP, FLATBED, PANEL, PICKUP, CONCRETE) - 15 FEET

• FRONT END LOADERS - 15 FEET

• DOZERS - 15 FEET

• CRANE - 25 FEET \*

• BATCH PLANT - 60 FEET

• ROLLERS AND COMPACTORS - 15 FEET
- \*CONSTRUCTION EQUIPMENT LOCATIONS SHALL NOT VIOLATE RUNWAY 7:1 TRANSITIONAL SURFACES AND RUNWAY APPROACH ZONE HEIGHT LIMITATIONS EXCEPT UNDER SPECIAL WAIVER CONDITIONS. APPROPRIATE WAIVERS MUST BE OBTAINED BY THE OWNER FROM FAA. CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS 48 HOURS PRIOR TO USING CRANE.
- \*ALL TEMPORARY EQUIPMENT USED MUST BE EVALUATED IN OE/AAA THOUGH THE FAA, REGARDLESS OF THEIR HEIGHTS.
- | CONSTRUCTION SCHEDULE |                  |                               |
|-----------------------|------------------|-------------------------------|
| PHASE                 | DURATION         | ANTICIPATED NOTICE-TO-PROCEED |
| ADMINISTRATIVE        | 30 CALENDAR DAYS | AUGUST 2025                   |
| AIRFIELD CLOSURE      | 10 CALENDAR DAYS | SEPTEMBER 2025                |
- RS&H

2600 Park Tower Dr., Suite 101

Vienna, Virginia 22180

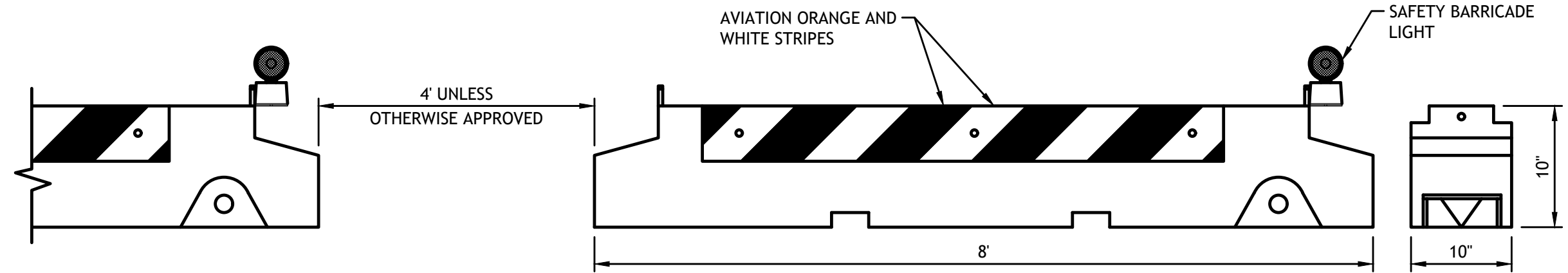
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Virginia Registration Nos. 0407-003171 \* 0411-000438
- 
- CHARLOTTESVILLE  
ALBEMARLE  
AIRPORT  
(CHO)
- RUNWAY 3-21 AND  
TAXIWAY A  
PAVEMENT  
MAINTENANCE
- | REVISIONS |             |      |
|-----------|-------------|------|
| NO.       | DESCRIPTION | DATE |
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|---------------------------------------|------------|
| DATE ISSUED:                          | MARCH 2025 |
| REVIEWED BY:                          | CCA        |
| DRAWN BY:                             | LSB / RKH  |
| DESIGNED BY:                          | RSY        |
| RS&H PROJECT NUMBER                   |            |
| 10541892013                           |            |
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| SHEET TITLE                           |            |
- CONSTRUCTION  
SAFETY  
AND PHASING  
NOTES
- |               |
|---------------|
| SHEET NUMBER  |
| G005          |
| SHEET 5 OF 19 |
- BID DOCUMENTS



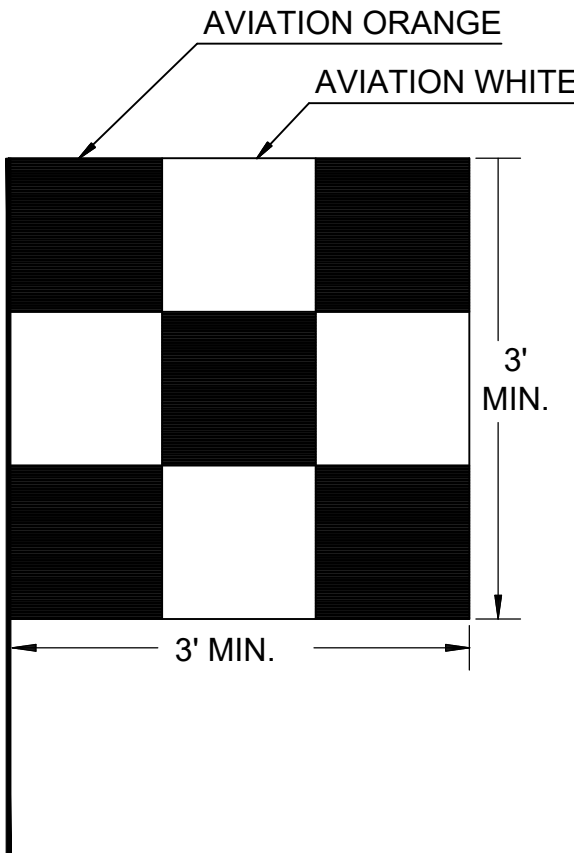
Drawing: P:\Charlottesville\_Albemarle\10221892\_CHO\_Civil\_On\_Call\_2022\_2026\10541892013\03.00 Project Execution\03.05 Dwggs\_Models\CAD\CHO-PMT-Constr Safety Security Notes Details.dwg - Plotted on: 5/15/25 - Plotted by: Blaine, Jyson



1  
G006 TYPE 1 BARRICADE DETAIL  
SCALE: N.T.S.

BARRICADE NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING BARRICADES, BARRICADE LIGHTS, AND SANDBAGS/ANCHORS, AS WELL AS PLACEMENT, MAINTENANCE, AND REMOVAL OF BARRICADES. ALL MATERIALS, PLACEMENTS, TRANSPORT, AND MAINTENANCE OF BARRICADES SHALL BE INCIDENTAL TO X-102, SAFETY AND SECURITY.
2. BARRICADES SHALL MEET THE REQUIREMENTS OF FAA AC 150/5370-2G, OR LATEST REVISION.
3. THE CONTRACTOR SHALL MAINTAIN PROPER POSITIONING OF ALL BARRICADES, AS WELL AS ENSURE THE BARRICADES REMAIN FULL OF WATER.
6. THE CONTRACTOR SHALL CHECK OPERABILITY OF LIGHTS DAILY. HAVE REPLACEMENT LIGHTS AND BATTERIES ON-SITE AND REPLACE INOPERATIVE LIGHTS WITHIN ONE (1) HOUR OF NOTIFICATION OF OUTAGE BY THE ENGINEER/RPR OR AIRPORT OPERATIONS. PROVIDE AN ON-SITE REPRESENTATIVE WHO IS AVAILABLE 24-HOURS PER DAY, SEVEN DAYS PER WEEK AND CAN REPLACE INOPERATIVE LIGHTS.
7. SANDBAGS AND/OR ANCHORS MAY BE REQUIRED TO HOLD THE BARRICADES IN PLACE AS THE BARRICADES WILL BE EXPOSED TO JET BLAST AND PROP WASH. SANDBAGS AND ANCHORS ARE INCIDENTAL TO X-102, SAFETY AND SECURITY.
8. MAINTENANCE AND SECURING OF BARRICADES IN PLACE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THOUGH MULTIPLE INSTALLATIONS AND REMOVALS OF THE BARRICADES MAY BE REQUIRED, THE CONTRACTOR WILL BE PAID ONLY FOR THE INITIAL INSTALLATION OF EACH BARRICADE. ALL WORK ASSOCIATED WITH THE INSTALLATION AND MAINTENANCE OF LOW PROFILE BARRICADES SHALL BE INCIDENTAL TO X-102, SAFETY AND SECURITY. NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE.
9. BARRICADES SHALL REMAIN PROPERTY OF THE CONTRACTOR AT PROJECT COMPLETION.



2  
G006 CONSTRUCTION SAFETY FLAG  
SCALE: NTS

RS&H

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Vienna, Virginia 22180  
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Virginia Registration Nos. 0407-003171 \* 0411-000438



CHARLOTTESVILLE  
ALBEMARLE  
AIRPORT  
(CHO)

RUNWAY 3-21 AND  
TAXIWAY A  
PAVEMENT  
MAINTENANCE

REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: MARCH 2025  
REVIEWED BY: CCA  
DRAWN BY: LSB / RKH  
DESIGNED BY: RSY

RS&H PROJECT NUMBER  
10541892013  
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CONSTRUCTION  
SAFETY  
AND PHASING  
DETAILS

SHEET NUMBER  
G006  
SHEET 6 OF 19

BID DOCUMENTS